

 <b>Reigate &amp; Banstead</b> BOROUGH COUNCIL Banstead   Horley   Redhill   Reigate	<b>TO:</b>	PLANNING COMMITTEE
	<b>DATE:</b>	18 March 2020
	<b>REPORT OF:</b>	HEAD OF PLANNING
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<b>AGENDA ITEM:</b>	7	<b>WARD:</b> <i>Hooley, Merstham &amp; Netherne</i>

<b>APPLICATION NUMBER:</b>	19/01890/F	<b>VALID:</b>	02/10/2019
<b>APPLICANT:</b>	Carvall Homes Ltd	<b>AGENT:</b>	Grainger Planning Associated Ltd
<b>LOCATION:</b>	134 BRIGHTON ROAD, HOOLEY		
<b>DESCRIPTION:</b>	<b>Demolition of the existing chalet-style dwelling and the erection of a development of 5 flats in a two storey building with roof accommodation together with the provision of refuse and recycling stores and five car parking spaces.</b>		
	<b>All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail</b>		

**This application is referred to Committee in accordance with the Constitution as the application site is for net 4 dwellings**

## **SUMMARY**

The proposed development comprises the demolition of the existing house and the erection of a part single, part 1 ½ and part 2 ½ storey block of 5 flats, with ancillary parking, amenity space and landscaping.

The surrounding area is predominantly residential in character and comprises a range of dwelling types, sizes and designs with another flatted scheme to the north of this site. The site fronts onto the busy A23.

The proposed development would have a traditional design with the front part of the block being 2 ½ storeys in height with rooms in the roofspace. The building would be larger than the existing dwelling but has been amended during the course of the application to reduce its scale; improve its built relationship to the adjacent dwellings; amend the proximity to site boundaries, and; take into account the topography of the site. It is thereby considered that the proposal, as amended, would fit within and respect the character of this area without adversely affecting the amenities of neighbouring properties.

The site lies in an area of low accessibility to nearby facilities and services but lies overall in a sustainable location by virtue of the availability of access to public transport that provides access to Redhill, Coulsdon and Croydon with their wider range of services and facilities.

Parking provision would be one space per flat but as a result of the availability of public transport to future residents, the prevention of on street parking immediately outside the site by the existence of double yellow lines on the A23 it is considered that the scheme would not cause harm to either highway safety or the free flow of traffic. Accordingly, the scheme has been considered by Highways England and the County Highways Authority and whilst the proposed on-site car parking is less than the required standard, subject to recommended conditions, the scheme is considered acceptable.

### **RECOMMENDATION**

Planning permission is **GRANTED** subject to conditions.

## **Consultations:**

### County Highway Authority:

The County Highways Authority (CHA) advise that, on balance the, application is supported from a sustainable transport development perspective. The CHA note that the site is not within walking distance of education, employment, retail and leisure land uses. However, there are bus service within 400 metres of the site serving buses going south and north bound along the A23. These buses serve Croydon and Redhill that do have a wider range of these land uses and have train stations that serve more London locations with a larger range of these land uses.

No objection is raised on safety, capacity and policy grounds subject to the imposition of a number of conditions relating to the provision of the approved access, provision of visibility splays, parking provided prior to occupation, turning space prior to occupation, CTMP, provision of cycle parking and provision of a fast charge socket on at least one space.

With regards to parking, there are parking restrictions on the highway, that would prevent inappropriate parking on the footway and carriageway. A condition is proposed that requires measures to prevent parking on the verge.

### Highways England:

No objection to the development subject to conditions to conditions to prevent loading/unloading or parking of construction vehicles at any time for the duration of the works , and; that there be no obstruction of the highway (to include footway and carriageway) to ensure that there is no disruption the Strategic Road Networks (SRN) in the interests of the safe and efficient operation of the SRN.

It is noted that there are double yellow lines in place and there is no parking permitted at any time, and at present no loading Mon-Sat 8-9.30AM and 4.30-6.30PM. However, notwithstanding the present loading/unloading restrictions Highways England are, as per the requested condition looking for there to be no loading/unloading or parking of construction vehicles at any time for the duration of the works.

## **Representations:**

Letters were sent to the adjoining neighbours and those opposite and a site notice was posted. No comments were received.

### **1.0 Site and Character Appraisal**

1.1 The site comprises a detached 2 storey dwelling with a mansard roof lying in an area of residential development that fronts onto the A23, close to the junction with the M23. The house sits between a chalet bungalow with detached garage lying adjacent to the shared boundary to the south and a two-storey house to the north which has rooms in the roof with a similar roof style to that now proposed on the application site. The application site has a parking area in front of the house and a front parking area that is dominated

by two existing conifers. The house has a brick wall adjacent to the boundary with the pavement.

- 1.2 Land levels vary around this area with the houses on and around the application site being broadly level with the highway and with rear gardens that rise relatively steeply away from the house. In the case of the application site, steps lead up to a rear garden that is higher than the patio. Houses on the opposite side of this part of the A23 are set at a significantly higher level than the highway with front gardens that rise quite steeply away from the highway. Further to the north the land levels out with houses on both sides of the road being broadly level with the road.
- 1.3 The site fronts onto the A23 which at this point has double yellow lines in place and no parking is permitted at any time, and no loading Mon-Sat 8-9.30AM and 4.30-6.30PM. The nearest on-street parking is in a side road that runs parallel to the A23 to the south around the Star Bucks Coffee Shop on the corner of the A23 and Dean Lane or the residential roads that run parallel to the A23 to the rear of the site.
- 1.4 The surrounding area is characterised by a variety of dwelling types, designs and sizes. All have front gardens large enough to provide at least some off-street parking, although not all gardens do so. The front gardens are of differing characters, some wholly planted, some with mixed planting and parking and some wholly parking.

## **2.0 Added Value**

- 2.1 Improvements secured at the pre-application stage: Advice given to suggest a smaller scheme than had been submitted with the removal of windows facing neighbouring gardens, lowering of the roofline and reduction of the rearward projection.
- 2.2 Improvements secured during the application: Reduction of the scale and extent of development, removal of one flat and changes to fenestration. Further information secured regarding the highways concerns expressed by the Highways authorities.
- 2.3 Further improvements to be secured through conditions regarding landscaping and management of vehicular access to the site.

## **3.0 Relevant Planning and Enforcement History**

- 3.1 None

## **4.0 Proposal and Design Approach**

- 4.1 The scheme comprises the demolition of the existing house and its replacement with a building featuring two floors of accommodation and an additional floor with accommodation in the roofspace. This would provide for 4 x 2-bedroom flats and 1 x 1 bedroom flat. The existing drive would be used to provide off street parking and turning space with a landscaped frontage proposed along the boundary of the site with the pavement.
- 4.2 The building would have a traditional appearance with a pitched tiled roof and brick elevations. The front elevation would feature two x two storey projecting bay windows – one each side of the centrally placed entrance door with tile hanging between the ground and first floor bays windows and some additional tile hanging in the front gable. The front roofspace would accommodate a small pitched roof dormer window placed above one of the projecting bays. The rear would feature a part single, part 1 ½ storey addition with a double pitched tiled roof and a single storey flat roof addition, both inset from the full width of the main part of the block. The 1 ½ storey rear addition would project 4.2m's beyond the main part of the building, with the single storey rear extension a further 2m's. It would be inset from the adjacent garden boundaries by 2.8m's from the boundary with the dwelling to the south and 3m's from the boundary with the house to the north.
- 4.3 The rear garden slopes uphill away from the existing house and would be excavated out to accommodate the rear additions at the same ground floor level as the main part of the house and the neighbouring houses. Sufficient space would be allowed around the rear addition to allow pedestrian access and two sets of steps would be provided to gain access to the rear garden. The existing garden levels would be retained adjacent to the existing boundary fence and the neighbouring gardens. The level change of the rear garden is such that it would almost be level with the roof of the single storey rear addition and with the retention of boundary fences on both sides this element of the works should not project above the level of the top of the fences.

Site area	0.06 ha
Existing use	Residential House
Proposed use	5 apartments
Existing parking spaces	Approximately 3/4 spaces
Proposed parking spaces	5
Parking standard	10
Net increase in dwellings	4
Infrastructure contribution	CIL - £140/sq m
Density of existing site	13dph
Density of proposed site	65dph
Density of the surrounding area	25dph

## Policy Context

### 5.1 Designation

Urban Area

Surface water flooding 1:30,1:100 1:1000 year event along the adjacent highway but not the site

Low accessibility rating

### 5.2 Reigate & Banstead Borough Core Strategy:

CS1, Presumption in favour of sustainable development

CS10. Sustainable development

CS11, Sustainable construction

CS17, Travel options and accessibility

### Reigate and Banstead Development Management Plan 2019

DES1, Design of new development

DES4 Housing Mix

DES5 Delivering High Quality Homes

DES8 , Construction management

TAP1 Access, parking and servicing

NHE2 Protecting and enhancing bio diversity and areas of geological importance

NHE3 Protecting trees, woodland areas and natural habitats

### 5.3 Other Material Considerations

National Planning Policy Framework

Supplementary Planning Guidance

Surrey Design

Local Distinctiveness Design Guide

Householder Extensions and Alterations

Other

Human Rights Act 1998

### 6.0 **Principal Issues**

6.1 The main issues to consider are:

- Design appraisal
- Neighbour amenity
- Quality of Accommodation
- Access and parking
- Trees and landscape

### Design Appraisal

- 6.2 Policy DES1 expects new development to be of a high-quality design that makes a positive contribution to the character and appearance of its surroundings. It should, amongst other requirements, promote local distinctiveness, use high quality materials, have regard to the relationship with neighbouring buildings, and incorporate appropriate landscaping.
- 6.3 The design of the scheme is of a traditional form, with a pitched tiled roof, brick and tile hung elevations, projecting bay windows and generally domestic scale architecture. The front elevation would present an attractive elevation to the streetscene and would have the character of a larger house and would fit within the character of the surrounding area with its many different building styles and sizes.
- 6.3 The design of the building does include a crown roof however from the highway the crown roof would not be harmful and the depth of the building would be partially obscured by the adjacent dwellings. The set back of the rear addition would provide a satisfactory breaking up of the mass of the building when viewed through gaps between the proposed building and its neighbours. The use of the roofspace for accommodation is a feature of this area already with the adjacent house to the north of the site (amongst others) having the same type of roof as that proposed on this site.
- 6.5 The proposal does include an increase in the car parking capacity in the front garden and the provision of a bin storage area/cycle storage area. However this would not adversely affect the streetscene such as to cause harm. To facilitate the additional car parking the two large evergreen trees would be removed with a consequent visual impact upon the streetscene. However, they are over large for their position resulting in the overshadowing of the front of the dwelling and they are not of such quality or amenity value as to justify formal protection by service of a Tree Preservation Order. To compensate there is proposed some landscaping at the front of the site.
- 6.6 Policy DES1 advises that all new development will be expected to, amongst other criteria, have due regard to the....density.... of the surrounding area.
- 6.7 The NPPF encourages the effective use of land, but does not specify specific density targets.
- 6.8 The density of a proposed scheme is one measure of potential impacts of a scheme, but does not alone indicate that a scheme is unacceptable or indeed acceptable – that consideration must take account of a wide range of factors. In this instance the density figures outlined in Section 4. of this report suggests a density range that would be significantly in excess of surrounding sites. However this is because, very simply the figure compares an area of single family homes with a single site with 5 units. If the proposed scheme is compared to a site some 170 m's to the north of the application site (The Sycamores) which is a development of 2 block of flats, then it has a comparable density to that development.

- 6.9 Therefore whilst density is an indication of potential impacts upon the surrounding area, the scheme must be considered in more detail to assess if there are any adverse impacts arising from a significantly higher density on this one site. Overall whilst a different appearance and density would result, this is not considered harmful or contrary to those policies the guide development in the context of the character of the area.

Neighbour Amenity:

- 6.10 Policy DES1 seeks to ensure that new development does not adversely impact upon the amenities of occupiers of nearby buildings.
- 6.11 The scheme would lie between a two-storey dwelling and a chalet bungalow with broadly the same front building line as the proposed scheme. The rear part of the building would be set back 4.6m's behind the rear of the adjacent dwellings at 1 1/2 storey level and a further 2m's at single storey level. In terms of the neighbours privacy, the scheme has been amended to ensure that the sideward facing windows above the ground floor level would be primarily to bathroom, but with one first floor kitchen window facing towards the neighbouring chalet bungalow. The bathroom windows can be obscured glazed whilst the kitchen window would lie in such a position along the depth of the property as to look over the roof line of the chalet bungalow rather than into any windows. It is not considered that this would therefore result in any material loss of privacy to the neighbouring properties.
- 6.13 In terms of the rearward projection of the scheme, the main impact in terms of overshadowing would be to the adjacent house lying to the north of the application site. The 1 1/2 storey rear addition to this scheme would lie to the rear of the adjacent houses, but the combination of the set back of the adjacent house from the shared garden boundary (3.6m's) and the set back of the 1 1/2 storey element from the shared boundary (3 m's) would mean that there would not be a materially harmful impact to either the rear windows of the neighbouring houses or to their gardens. Other windows to the proposed flats would be rear facing and thus would have similar levels of overlooking as most dwellings in respect of overlooking of neighbouring gardens.
- 6.14 As a guide to the impacts of such development upon neighbouring property light and sunlight the Councils SPG on Householder Extensions and Alterations addresses this issue. It advises that where an extension exceeds a 45-degree line plotted from the middle of the neighbours ground floor window to the rear corner of the proposed extension, that it may significantly affect light into the neighbours habitable rooms. In this case the 45-degree line 'test' is not contravened in respect of either adjacent property.
- 6.15 Visually the scheme would represent a different outlook compared to the current situation to both neighbouring sites. However as a combination of amendments made to incorporate the first floor of the rear addition into the roofspace, the changing garden levels on the adjacent plots, the separation between the proposed works and the house to the north and the separation

and intervening single storey outbuildings in the garden of the chalet bungalow to the south, it is not considered that the impacts would be so visually intrusive as to cause significant harm and thus be unacceptable.

- 6.16 Overall whilst the scheme would represent a different outlook to the adjacent neighbours, it is considered that the scheme has been amended such as to overcome initial concerns and reduce the impacts to an acceptable level that would be policy compliant.

#### Quality of Accommodation

- 6.17 Policy DES5 requires all new residential development to provide high quality accommodation which must meet the relevant nationally described internal space standards for each unit. It further requires that habitable rooms have an acceptable outlook and where possible receive direct sunlight and provide a convenient and efficient layout including sufficient circulation space and avoiding awkward or impractically shaped rooms.
- 6.18 The proposed scheme would provide 4 x 2-bedroom units and 1 x 1-bedroom unit arranged over three floors. The flats on the ground and first floors would be arranged with the bedrooms towards the front part of the building and the living rooms at the rear. Depending upon the size of the unit the kitchen is either integral with the living room or a separate room. The design of the units would provide a sensible and usable layout with no awkward corners or spaces that would be difficult to use. The window layout would provide satisfactory outlook and access to light/sunlight to all rooms.
- 6.19 Off street parking would be provided based on one space per unit which is less than the parking standard required. However, whilst this may be inconvenient to future residents the site does lie in a sustainable location in terms of its access to public transport and wider range of services. We are encouraged to encourage less reliance upon the private motor vehicle, and it is not considered that this could be harmful to the amenities of the future or surrounding residents. The rear garden would provide adequate shared amenity space. Overall it is considered that the scheme would provide a satisfactory standard of accommodation.

#### Access and Parking:

- 6.20 Policy TAP1 requires new development to provide safe and convenient access for all road users, not to materially exacerbate traffic congestion on the existing highways network, provide adequate access onto and within a site, include sufficient parking and cycle storage, include electric charging points, and to comply with current highways standards and guidance.
- 6.21 The site fronts onto the A23 which, due to its proximity to the M23 is the responsibility of Highways England and therefore consultations have taken place with both Highways England and the County Highway Authority. Both have expressed concerns regarding the potential impacts during the development of the site in terms of the movement of vehicles during

construction and their potential to cause highways safety issues as well as impeding the free flow of traffic on this busy stretch of highway.

- 6.22 Off street parking would be provided at a level of 1 space per unit. The parking standard identified in the Development Management Plan identifies that each of the two-bedroom units should have 2 spaces, the one bed flat 1 space and a visitor space. Only one space per unit is provided and on that basis the site would be 5 spaces short of the normal policy requirement.
- 6.23 The site lies in an area of low accessibility with limited nearby access to a range of facilities: within walking distance lie two petrol filling stations and a small parade of shops which currently accommodates a café, wine shop, newsagent, window shop and a vacant unit. A coffee shop lies approximately 2 minutes walk to the south east. The nearest other facilities are not within walking distance. A bus service runs through Hooley past the site giving access to Redhill and Coulsdon and Croydon with a significant range of facilities. The bus stop is within easy walking distance of the site. On this basis it is concluded that the site is in an accessible location.
- 6.24 In terms of off street parking, due to the double yellow lines outside the site and along most of the nearby A23, the nearest on-street parking spaces lie some 180m's to the south east along an access road to houses on the opposite side of the A23, that runs parallel to the A23 either side of Dean Lane and is in use for parking for local residents. Residential roads to the rear of the site offer other potential for on-street parking.
- 6.25 The scheme has been subject of considerable discussion with both Highways England and the County Highways Authority and both are now satisfied, subject to a number of restrictive conditions, that the scheme would be acceptable.
- 6.26 On the basis of one car space per unit and in the knowledge that the site is in a sustainable location with policies encouraging less reliance upon the private motor vehicle, it is not considered that the proposed parking level would be unacceptable and result in harm.

#### Trees and Landscape:

- 6.27 Policy NHE3 requires new development to include an assessment of trees on the site and an assessment of their suitability or retention. Unprotected but important trees should be retained as an integral part of the design of development and any such trees lost should be subject to compensatory planting.
- 6.28 At present two large conifers in the front garden of the house make a significant visual impact, but are overlarge for their position so close to the existing house and the roots would appear to be breaking up the paving on the driveway as well as overshadowing the front of the dwelling. They would need to be removed to facilitate the proposed parking scheme but are not protected. Two planting beds along the front boundary are proposed as well

as a small bed immediately in front of the new flats. Whilst they could not accommodate a tree of equal size as those being removed, there would be sufficient space for some planting that would soften the appearance of the scheme and contribute to the wider streetscene and this is addressed by means of an appropriate condition. It is noted that an existing tree that lies on the boundary of this site and the house to the north would be retained providing a larger scale tree at the front of the site.

- 6.29 Overall it is considered that the impacts of the scheme are acceptable in this respect.

## CONDITIONS

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.  
Reason:  
To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004.

2. The development hereby permitted shall be carried out in accordance with the following approved plans:

<b>Plan Type</b>	<b>Reference</b>	<b>Version</b>	<b>Date Received</b>
Proposed site plan	134BR P2	G	3.12.19
Plans and Elevations	134BR P3	F	12.12.19

3. The development shall not be first occupied until the amended access to 134 Brighton Road, with realigned dropped kerb, reinstatement of full height kerb and verge in place of closed access and installation of physical measures to prevent parking on the verge, hereby approved has been constructed and provided with a means within the private land of preventing private water from entering the highway at the back edge of the highway and visibility zones in accordance with the approved plans and thereafter the visibility zones shall be kept permanently clear of any obstruction over 0.6m high.

Reason:

In order that the development should not prejudice highway safety nor cause inconvenience to other highway users in accordance with the provisions of Policy TAP1 of the Reigate and Banstead Development Management Plan 2019.

4. The development hereby approved shall not be first occupied unless and until a pedestrian inter-visibility splay measuring 2m by 2m has been provided on each side of the access to 134 Brighton Road, the depth measured from the back of the footway (or verge) and the widths outwards from the edges of the access. No obstruction to visibility between 0.6m and 2m in height above ground level shall be erected within the area of such splays.

Reason:

In order that the development should not prejudice highway safety nor cause inconvenience to other highway users in accordance with the provisions of Policy TAP1 of the Reigate and Banstead Development Management Plan 2019.

5. The development hereby approved shall not be first occupied unless and until space has been laid out within the site in accordance with the approved plans for vehicles / cycles to be parked and for vehicles to turn so that they may enter and leave the site in forward gear. Thereafter the parking and turning area shall be retained and maintained for their designated purposes.

Reason:

In order that the development should not prejudice highway safety nor cause inconvenience to other highway users in accordance with the provisions of Policy TAP1 of the Reigate and Banstead Development Management Plan 2019.

6. No development shall commence until a Construction Transport Management Plan, to include details of:
- (a) parking for vehicles of site personnel, operatives and visitors
  - (b) loading and unloading of plant and materials
  - (c) storage of plant and materials
  - (d) programme of works (including measures for traffic management)
  - (e) provision of boundary hoarding behind any visibility zones
  - (f) Construction vehicle deliveries and hours of operation
  - (g) vehicle routing
  - (h) measures to prevent the deposit of materials on the highway
  - (i) before and after construction condition surveys of the highway and a commitment to fund the repair of any damage caused
  - (j) Construction vehicle movements to or from the site shall only take place between the hours of 9.15 am and 4.45 pm, to avoid impacting on the strategic operation of the A23 Brighton Road at peak network times, nor shall the contractor permit any construction vehicles associated with the development at the site to be laid up, waiting in or on the verge of Brighton Road, outside of these times
  - (k) on-site turning for construction vehicles has been submitted to and approved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development.
  - (L) details and provisions to control and manage construction traffic and measures to prevent dust and debris from being blown or otherwise deposited onto the A23,

has been submitted to and approved in writing by the Local Planning Authority (who shall consult with Highways England) and the County highways Authority. The construction of the development shall be carried out in accordance with the approved Construction Management Plan.

Reason:

To ensure that construction of the development does not result in avoidable congestion of the A23, to ensure that access/egress at the development, in so far as it affects the A23 trunk road, is maintained in a safe manner and

that the site is managed in a manner to prevent extraneous material being deposited on the highway. This is to ensure that the A3 Trunk Road continues to be an effective part of the national system of routes for through traffic in accordance with section 10 of the Highways Act 1980 and to satisfy the reasonable requirements of road safety and to ensure compliance with Policy TAP1 of the Reigate and Banstead Development Management Plan 2019.

7. No operations involving the bulk movement of materials to or from the development site shall commence unless and until facilities have been provided in accordance with a scheme to be submitted to and approved in writing by the Local Planning Authority to, so far as is reasonably practicable to prevent the creation of dangerous conditions for road users on the public highway. The approved scheme shall thereafter be retained and used whenever the said operations are undertaken.

Reason:

In order that the development should not prejudice highway safety nor cause inconvenience to other highway users in accordance with the provisions of Policy TAP1 of the Reigate and Banstead Development Management Plan 2019.

8. The development hereby approved shall not be first occupied unless and until the following facilities have been provided in accordance with the approved plans for:

- (a) The secure, level and covered parking of six bicycles within the development site,

- (b) Information to be provided to residents regarding the availability of and whereabouts of local public transport, and thereafter the said approved facilities shall be provided, retained and maintained to the satisfaction of the Local Planning Authority.

Reason:

In recognition of Section 9 "Promoting Sustainable Transport " in the National Planning Policy Framework 2018 and the provisions of Policy TAP1 of the Reigate and Banstead Development Management Plan 2019.

9. The development hereby approved shall not be occupied unless and until at least one of the available parking spaces has been provided with a fast charge socket (current minimum requirement: 7kw Mode 3 with Type 2 connector - 230 v AC 32 amp single phase dedicated supply) in accordance with a scheme to be submitted to and approved in writing by the Local Planning Authority.

Reason:

In recognition of Section 9 "Promoting Sustainable Transport" in the National Planning Policy Framework 2018 and the provisions of Policy TAP1 of the Reigate and Banstead Development Management Plan 2019.

10. At no time shall any construction vehicles connected with the development of this site (including demolition) park or wait on the A23 outside of, or within close proximity to the property.

Reason:

To ensure no adverse impact upon either highway safety or the free flow of traffic on the adjacent highway resulting from the development of this site in accordance with the provisions of Policy TAP1 of the Reigate and Banstead Development Management Plan 2019.

11. No development above slab level shall take place until samples of the materials to be used in the construction of the external surfaces, including fenestration and roof, have been submitted to and approved in writing by the Local Planning Authority, and on development shall be carried out in accordance with the approved details.

Reason:

To ensure that a satisfactory external appearance is achieved of the development with regard to Policy DES1 of the Reigate and Banstead Development Management Plan 2019.

12. No development above slab level shall commence until all details of hard and soft landscaping have been submitted to and approved in writing by the local planning authority (LPA). These shall include all relevant scaled plans of hard & soft landscape existing & new, tree removal, level changes, other external works not dealt with elsewhere including boundary treatments, plus soft landscape establishment maintenance schedules for a minimum of 2 years. Soft landscaping shall include full planting specifications, planting sizes & densities. These shall all be coordinated with any tree protection requirements where required. All these works shall be carried out in strict accordance with these details as approved or as otherwise agreed in writing by the LPA and before occupation or use of this development plus thereafter the provision of the agreed establishment maintenance.

Reason:

To ensure good landscape practice in the interests of the maintenance of the character and appearance of the area and to comply with policy PC4 of the Reigate and Banstead Borough Local Plan 2005.

13. Prior to first occupation details shall be submitted to the Local Planning Authority of the proposed refuse storage area. The scheme shall be carried out in accordance with the approved details.

Reason:

To ensure satisfactory appearance upon completion in accordance with the provisions of Policy DES1 of the Reigate and Banstead Development Management Plan 2019.

14. The proposed flat roof area shall not be use as a terrace or other amenity area without the prior written approval of the Local Planning Authority.

Reason;

To protect the privacy and amenities of the adjacent residents in accordance with the provisions of Policy DES1 of the Development Management Plan 2019

Note: Should alterations or amendments be required to the approved plans, it will be necessary to apply either under Section 96A of the Town and Country Planning Act 1990 for non-material alterations or Section 73 of the Act for minor material alterations. An application must be made using the standard application forms and you should consult with us, to establish the correct type of application to be made

## **INFORMATIVES**

1) Details of the highway requirements necessary for inclusion in any application seeking approval of reserved matters may be obtained from the Transportation Development Planning Division of Surrey County Council.

2)The permission hereby granted shall not be construed as authority to obstruct the public highway by the erection of scaffolding, hoarding or any other device or apparatus for which a licence must be sought from the Highway Authority Local Highways Service.

3)The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damage the highway from uncleaned wheels or badly loaded vehicles. The Highway Authority will seek, wherever possible, to recover any expenses incurred in clearing, cleaning or repairing highway surfaces and prosecutes persistent offenders. (Highways Act 1980 Sections 131, 148, 149).

4) The permission hereby granted shall not be construed as authority to carry out any works on the highway or any works that may affect a drainage channel/culvert or water course. The applicant is advised that a Section 278 agreement must be obtained from the Highway Authority before any works are carried out on any footway, footpath, carriageway, verge or other land forming part of the highway. All works on the highway will require a permit and an application will need to be submitted to the County Council's Street Works Team up to 3 months in advance of the intended start date, depending on the scale of the works proposed and the classification of the road. Please see <http://www.surreycc.gov.uk/roads-and-transport/road-permits-and-licences/the-traffic-management-permit-scheme>. The applicant is also advised that Consent may be required under Section 23 of the Land Drainage Act 1991. Please see [www.surreycc.gov.uk/people-and-community/emergency-planning-and-community-safety/floodingadvice](http://www.surreycc.gov.uk/people-and-community/emergency-planning-and-community-safety/floodingadvice).

5) It is the responsibility of the developer to ensure that the electricity supply is sufficient to meet future demands and that any power balancing technology is in place if required. Please refer to: <http://www.beama.org.uk/resourceLibrary/beama-guide-to-electric-vehicle-infrastructure.html> for guidance and further information on charging modes and connector types.

## **REASON FOR PERMISSION**

The development hereby permitted has been assessed against development plan policies CS1, CS5, CS1, CS10, CS11, DES1, DE4, DES5, DES8, TAP1, NHE2, NHE3, and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.





**STREET SCENE** scale 1:200



**LOCATION PLAN**  
SCALE 1:1000



**SITE PLAN**

scale 1:100



**GRAHAM RIX R I B A**  
CHARTERED ARCHITECT  
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Proposed Development  
at  
**134 BRIGHTON ROAD**  
**HOOLEY CR5 3EF**  
for  
Carvall Homes Ltd

**PROPOSED  
SITE PLAN**

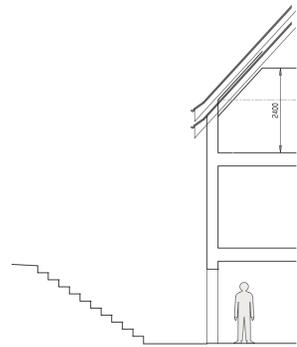
scale 1:100 & 1:200

09/09/2019

Rev G 03/12/2019 Change in roof plan to reflect reduced building  
Rev F 03/12/2019 cycle storage revised  
Rev C 28/11/2019 Dormer reduced  
Rev B 26/11/2019 Main ridge line reduced  
Rev A 25/11/2019 Roof plan revised and 5 parking spaces

**134BR**

**P2 (G)**



**FRONT ELEVATION**

scale 1:100



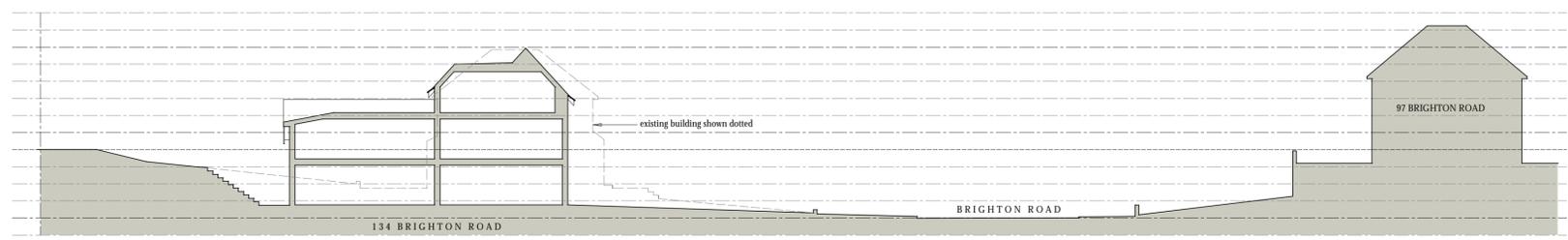
**FLANK ELEVATION**



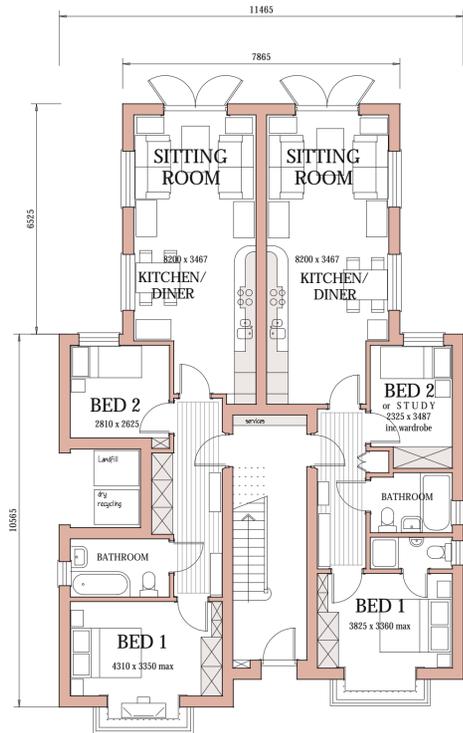
**REAR ELEVATION**



**FLANK ELEVATION**



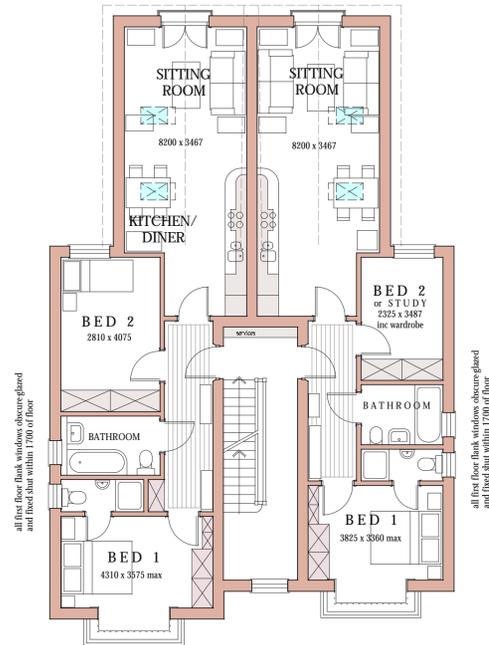
**SECTION THROUGH 134 & 97 BRIGHTON ROAD**



FLAT 1 61.9 sq m      FLAT 2 61.4 sq m

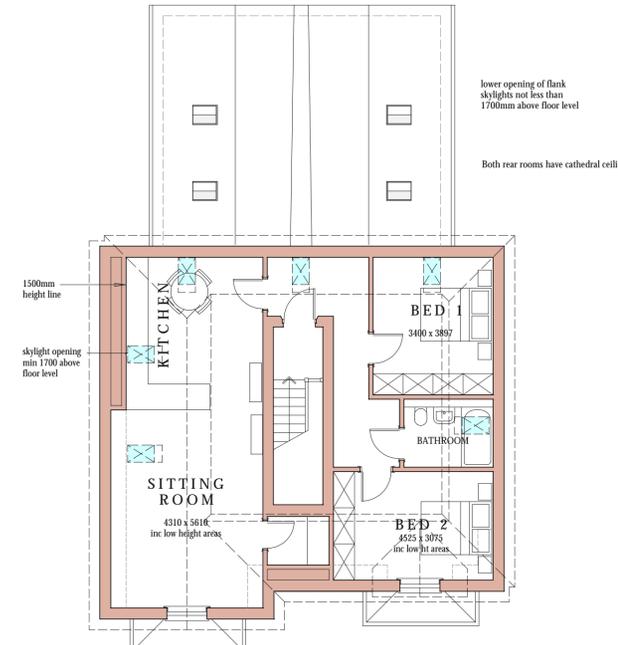
**GROUND FLOOR PLAN**

scale 1:100



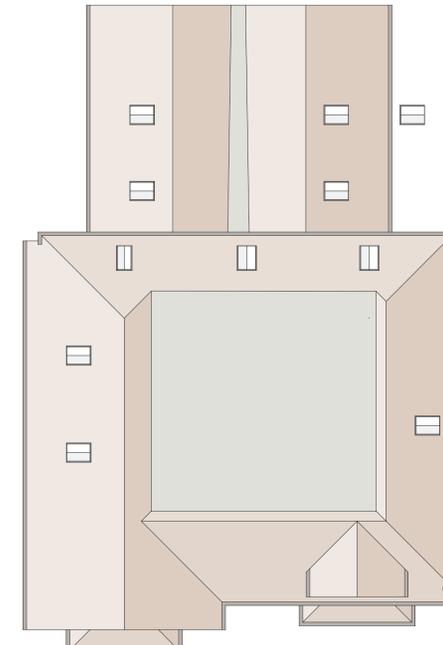
FLAT 3 68.4 sq m      FLAT 4 61.4 sq m

**FIRST FLOOR PLAN**



FLAT 5 83.9 sq m

**SECOND FLOOR PLAN**



**ROOF PLAN**



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**PLANS & ELEVATIONS**

scale 1:100      09/09/2019

Rev E 26/11/2019 Revised dormer  
Rev D 26/11/2019 Main ridge lowered  
Rev C 25/11/2019 Rear wing further reduced in height  
Rev B 13/11/2019 Rear wing reduced in height  
Rev A 14/09/2019 Finials removed

**134BR**      **P3** (E)

